



# Transport Access Program **Bexley North Station Upgrade** Determination Report



*Artist's impression of the proposed Bexley North Station Upgrade, subject to detailed design.*

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## Document control

<b>Status:</b>	Final
<b>Date of issue:</b>	April 2019
<b>Document author:</b>	Erin Fitzner, Jarryd Barton
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# Glossary and abbreviations

Term	Meaning
<b>CBD</b>	Central Business District
<b>CCTV</b>	Closed-circuit television
<b>CEMP</b>	Construction Environmental Management Plan
<b>CoA</b>	Condition of Approval
<b>Concept design</b>	The concept design is the preliminary design presented in the REF, which would be refined by the Contractor (should the Proposal proceed) to a design suitable for construction (subject to TfNSW acceptance)
<b>Contractor</b>	The Contractor for the Proposed Activity would be appointed by TfNSW to undertake the detailed design and construction of the Proposed Activity
<b>DDA</b>	<i>Disability Discrimination Act 1992 (Cwlth)</i>
<b>Detailed design</b>	Detailed design broadly refers to the process that the Contractor undertakes (should the Proposal proceed) to refine the concept design to a design suitable for construction (subject to TfNSW acceptance)
<b>DSAPT</b>	<i>Disability Standards for Accessible Public Transport (2002)</i>
<b>EP&amp;A Act</b>	<i>Environmental Planning and Assessment Act 1979 (NSW)</i>
<b>EP&amp;A Regulation</b>	<i>Environmental Planning and Assessment Regulation 2000 (NSW)</i>
<b>EPBC Act</b>	<i>Environment Protection and Biodiversity Conservation Act 1999 (Cwlth)</i>
<b>ISCA</b>	Infrastructure Sustainability Council of Australia
<b>LGA</b>	Local Government Area
<b>NES</b>	Matters of 'National Environmental Significance' under the EPBC Act
<b>NSW</b>	New South Wales
<b>OEH</b>	NSW Office of Environment and Heritage
<b>Proponent</b>	A person or body proposing to carry out an activity under Division 5.1 of the EP&A Act – in this instance, TfNSW
<b>Proposed Activity</b>	The construction and operation of the Bexley North Station Upgrade
<b>REF</b>	Review of Environmental Factors
<b>Scheduled rail shut down weekends</b>	Scheduled closures of Bexley North Station and/or the scheduled closure of the East Hills Rail Line during construction to accommodate certain works activities.
<b>SES</b>	NSW State Emergency Service
<b>SIMP</b>	Station Incident Management Plan

<b>Term</b>	<b>Meaning</b>
<b>SRZ</b>	Structural Root Zone
<b>TfNSW</b>	Transport for NSW (the Proponent)
<b>TGSI</b>	Tactile Ground Surface Indicators
<b>TPZ</b>	Tree Protection Zone

# Executive summary

## Overview of Proposed Activity

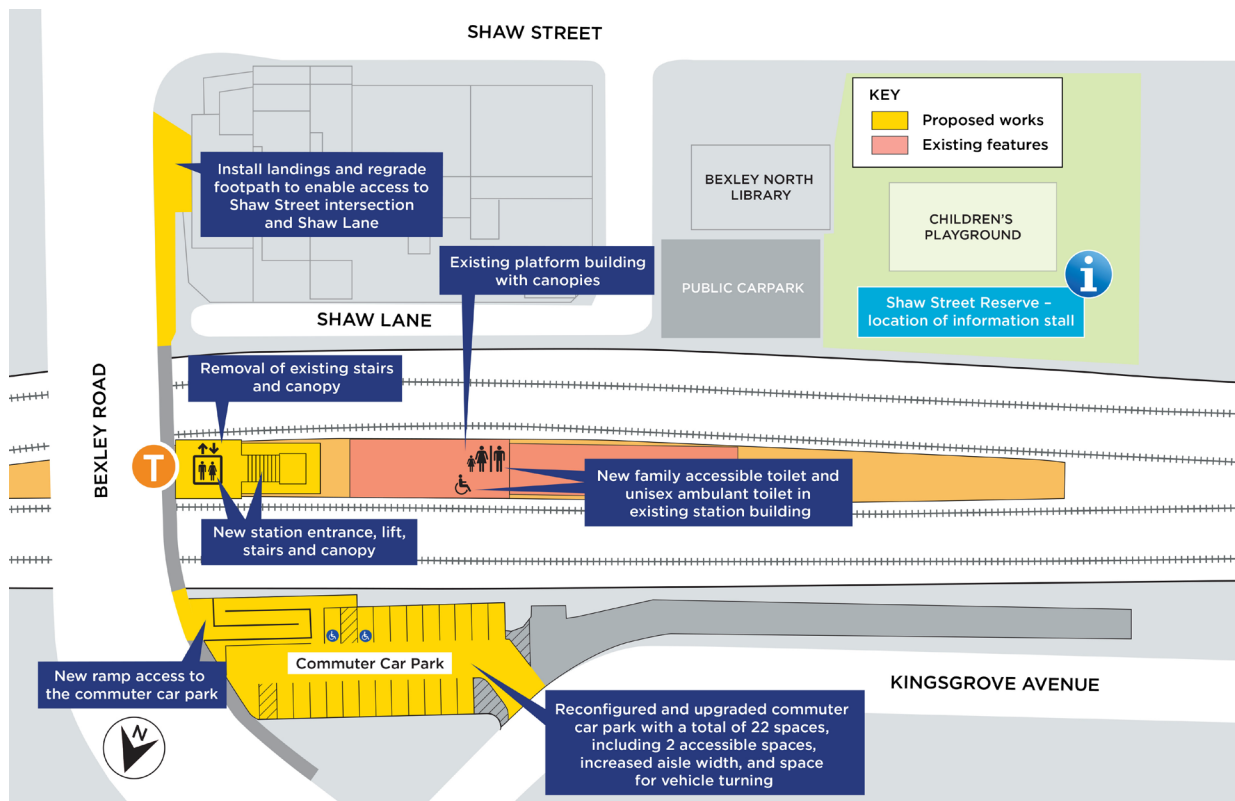
Transport for NSW (TfNSW) is responsible for improving the customer experience of transport services, transport policy and regulation, planning and program administration, procuring transport services and infrastructure and freight.

TfNSW is the Proponent for the Bexley North Station Upgrade (the Proposed Activity), which is part of the Transport Access Program. The program is a NSW Government initiative to provide a better experience for public transport customers by delivering accessible, modern, secure and integrated transport infrastructure. The program aims to provide a station precinct that is accessible to those with a disability, limited mobility, parents/carers with prams and customers with luggage.

In summary, the Proposed Activity as outlined in the REF would provide:

- a lift to the station platforms
- a new accessible path, ramp and stairs between the station and commuter car park on Kingsgrove Avenue
- improved amenities such as a new ambulant toilet and family accessible toilet.

An overview of the Proposal is shown below in Figure ES.1.



**Figure ES.1 Proposed Bexley North Station Upgrade (subject to change during detailed design)**

TfNSW, as the Proponent for the Proposed Activity, has undertaken a Review of Environmental Factors (REF) that details the scope of works and environmental impacts associated with the Proposed Activity. The REF was prepared by WSP Australia on behalf of TfNSW in accordance with the requirements of the *Environmental Planning and Assessment Act 1979* (EP&A Act) and clause 228 of the *Environmental Planning and Assessment Regulation 2000* (EP&A Regulation).

## **Modifications to the Proposed Activity**

No modifications have been made to the Proposed Activity since the REF was prepared, however a clarification of the site compound locations is made as follows:

- Section 3.2.7 of the REF (Ancillary Facilities) refers to two proposed construction compound locations, and that the *'impacts associated with utilising these areas have been considered as part of this environmental impact assessment'*. However, Section 6.1.2 of the REF (Traffic and Transport), refers to the main site compound being *'located in the commuter car park, north of the station or on the eastern side of Bexley Road'*.

It is likely both of these locations would be utilised simultaneously for a site compound and laydown area. As noted in Section 3.2.7 the impacts of both locations have been assessed and considered in the REF, and it is therefore not expected that there would be additional impacts as a result of both locations being used concurrently. Accordingly, no further mitigation measures are required.

Should design modifications be required as a result of the detailed design process, these modifications would be assessed to determine consistency with the Approved Project, including significance of impact on the environment. Additional mitigation measures and/or consultation would be undertaken where necessary.

## **Purpose of this report**

The purpose of this Determination Report is for TfNSW, as the Proponent of the Bexley North Station Upgrade, to determine whether or not to proceed with the Proposed Activity. TfNSW must make a determination in accordance with the provisions of Part 5, Division 5.1 of the EP&A Act.

This report also presents a summary of the submissions provided during the public display of the REF and TfNSW's response to the issues and comments raised in these submissions.

## **Conclusion**

Based on the assessments in the REF and a review of the submissions received from the community and stakeholders, it is recommended that the Proposed Activity be approved, subject to the mitigation measures included in the REF and the proposed Conditions of Approval. TfNSW will continue to liaise with the community and other stakeholders as the Proposed Activity progresses through detailed design and into the construction phase.

# 1 Introduction

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## 1.1 Background

The NSW Government is committed to facilitating and encouraging use of public transport, such as trains, by upgrading stations to make them more accessible, and improving interchanges around stations with other modes of transport such as buses, bicycles and cars. The Transport Access Program is an initiative targeted at achieving compliance with the *Disability Standards for Accessible Public Transport 2002* (DSAPT) Regulations across the network.

Bexley North Station has been identified for an accessibility upgrade as it currently does not meet key requirements of the DSAPT or the *Commonwealth Disability Discrimination Act 1992* (DDA).

The Proposed Action would fulfil the Transport Access Program objectives by proposing to provide:

- stations that are accessible to those with a disability, limited mobility, parents/carers with prams and customers with luggage
- modern buildings and facilities for all modes that meet the needs of a growing population
- safety improvements including extra lighting, lift alarm, fences and security measures for car parks and interchanges, including stations, bus stops and wharves
- signage improvements so customers can more easily use public transport and transfer between modes at interchanges

The Proposal is consistent with NSW planning strategies, including *NSW: Making It Happen* (NSW Government 2015) and the *Future Transport Strategy 2056* (TfNSW 2018). The Proposal would also ensure that Bexley North Station would meet legislative requirements under the DSAPT.

TfNSW is the Proponent for the Bexley North Station Upgrade (referred to as the 'Proposed Activity' for the purposes of this document).

## 1.2 Review of Environmental Factors

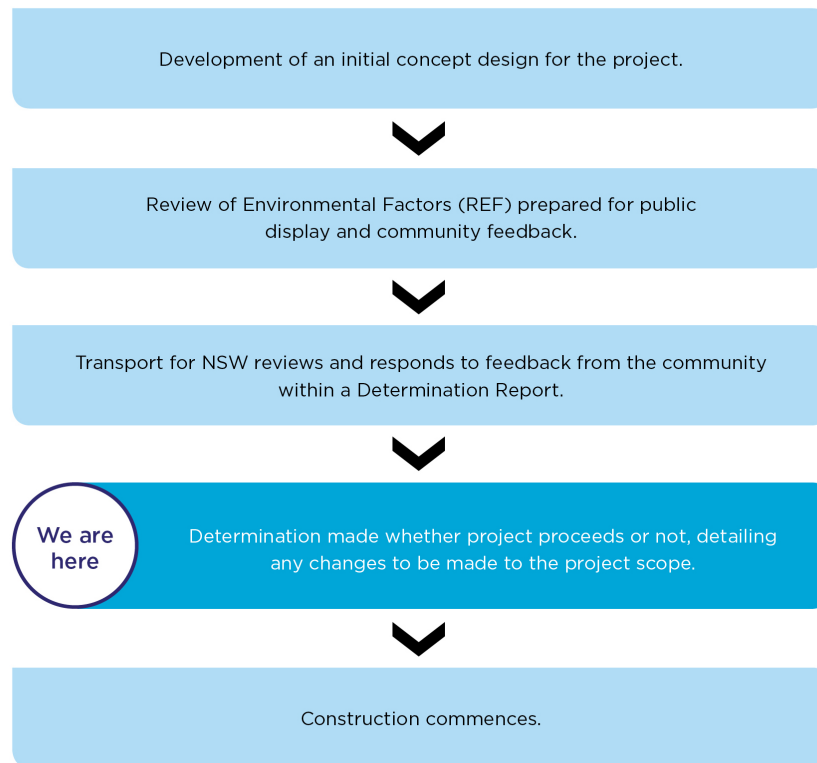
A Review of Environmental Factors (REF) was prepared by TfNSW in accordance with section 5.5 and section 5.7 of the *Environmental Planning and Assessment 1979* (EP&A Act), and clause 228 of the *Environmental Planning and Assessment Regulation 2000* (EP&A Regulation), to ensure that TfNSW takes into account to the fullest extent possible, all matters affecting or likely to affect the environment as a result of the Proposed Activity. The REF is included at Appendix A.

The Bexley North Station Upgrade REF was placed on public display from 8 February 2019 to 22 February 2019, with 18 submissions received. Issues raised in these submissions are addressed in Section 2.3 of this report.



### 1.3 Determination report

Prior to proceeding with the Proposed Activity, the Secretary for TfNSW must make a determination in accordance with Division 5.1 of the EP&A Act (refer Figure 1.1).



**Figure 1.1: Planning approval process**

The purpose of this Determination Report is to address the following to allow for a determination of the Proposed Activity:

- present a summary of the submissions received during the public display of the REF and TfNSW’s response to the issues and comments raised in these submissions
- assess the environmental impacts with respect to the Proposed Activity, which are detailed in the Review of Environmental Factors (and any proposed modifications, as detailed and assessed in this Determination Report)
- identify mitigation measures to minimise potential environmental impacts
- determine whether potential environmental impacts are likely to be significant
- address whether the provisions of the Commonwealth *Environment Protection and Biodiversity Conservation Act 1999* (EPBC Act) apply to the Proposed Activity.

This report has been prepared having regard to, among other things, the objectives of TfNSW under the *Transport Administration Act 1988*:

- a) to plan for a transport system that meets the needs and expectations of the public
- b) to promote economic development and investment
- c) to provide integration at the decision-making level across all public transport modes
- d) to promote greater efficiency in the delivery of transport infrastructure projects
- e) to promote the safe and reliable delivery of public transport and freight services.

## 1.4 Description of the Proposed Activity in the REF

Bexley North Station is located approximately 12 kilometres south west of Sydney's Central Business District (CBD). It is within the suburb of Bexley North and the Bayside Local Government Area (LGA). Bexley North Station does not currently meet key requirements of the DSAPT or the DDA. It also does not allow for equitable access to the station platforms.

An overview of the Proposed Activity, which is the subject of the Bexley North Station Upgrade REF, is provided in the Executive Summary with full details set out in Chapter 3 of the REF. In summary, the Proposed Activity, as outlined in the REF, comprises:

- provision of a new station entrance from the Bexley Road overbridge. The new station entrance would include:
  - demolition of the existing station access and the eastern platform canopy in order to accommodate the new lift and landing
  - construction of a new station entrance landing area
  - construction of a new lift between Bexley Road and the station platform
  - construction of new stairs between the proposed landing and the station platform
  - construction of a new platform level canopy which would wrap around and cover the new stairway
- internal station building works including:
  - construction of a new family accessible toilet in the location of the existing male toilets
  - construction of a new unisex ambulant toilet at the location of the existing female toilets
  - other minor building modifications that may be required to accommodate new electrical equipment including a main switchboard, and new or upgraded station communications equipment
- upgrade of existing platform surfaces (re-grading/re-surfacing) at locations across the platforms to provide compliant accessible paths and ramps to station amenities
- reconfiguration of the existing commuter car park on Kingsgrove Avenue including:
  - 22 parking spaces, including the provision of two compliant accessible parking spaces
  - an accessible ramp from the commuter car park to the Bexley Road overbridge
  - increased aisle width, space for vehicle turning and landscaping works
- upgrade of the existing footpaths to the north and south of the station entrance along the Bexley Road overbridge, including minor re-grading of footpaths and installation of landings
- ancillary works including adjustments to fencing, retaining walls, crash barriers, car park perimeter fence, lighting, electrical upgrades, relocation of rubbish bins, new and adjusted Opal card readers, improvements and modifications to station communications and security systems (including CCTV, public address system, hearing induction loops and station passenger information), wayfinding signage modifications and installation of tactile ground surface indicators.

The need for, and benefits of the Proposed Activity are outlined in Chapter 2 of the REF.

Construction is expected to commence during the second half of 2019 and take approximately 18 months to complete.

## 2 Consultation and assessment of submissions

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### 2.1 REF public display

The Bexley North Station Upgrade REF was placed on public display from 8 February 2019 to 22 February 2019, at three locations, as well as on the [TfNSW website](#)<sup>1</sup> and the [yoursay page](#)<sup>2</sup>. Community consultation activities undertaken for the public display included:

- community information stall on 12 February 2019 at Bexley North Station and Shaw Street Reserve
- distribution of around 2200 flyers
- installation of project signage at and around the station
- public display of the REF at:
  - Bayside Council – 444/446 Princes Highway, Rockdale
  - Bexley North Library – 24 Shaw Street, Bexley North
  - Transport for NSW Office – Level 5, Tower A, Zenith Centre, 821 Pacific Highway, Chatswood 2067.
- placement of information on the [TfNSW website](#)
- a briefing to Bayside Council officers on 7 February 2019.

### 2.2 REF submissions

A total of 18 submissions were received by TfNSW, including one from Bayside Council, one from the NSW State Emergency Service (SES) and 16 from the community. Of the 18 submissions, 14 expressed support for the Proposed Activity (78 per cent). The main issue raised by the submissions was the number of parking spaces to be provided in the upgraded commuter car park. Other issues raised were:

- the management of construction to minimise disruption and impacts on the surrounding environment
- project scope considerations, including requests for the inclusion of the following elements as part of the Proposed Activity:
  - additional seating
  - renewal of fencing
  - upgrade of the footpaths surrounding the station precinct
  - additional canopy coverage
- requests for the Proposed Activity to include provision of kiss-and-ride and bicycle parking facilities
- consultation with relevant authorities in the development of the Proposed Activity and the consultation strategy to be implemented during construction
- the design and management of the site to prepare for, respond to and recover from flooding events
- concern regarding the visual impact of the design on Bexley Road and the historical character of the station.

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<sup>1</sup> <https://www.transport.nsw.gov.au/projects/current-projects/bexley-north-station-upgrade>

<sup>2</sup> <http://www.yoursay.transport.nsw.gov.au>

## 2.3 Consideration and response to submissions

### Community submissions

A summary of all issues raised in community submissions, and TfNSW's responses to these issues is provided in Table 2.1.

**Table 2.1: Response to community submissions received**

No	Stakeholder	Issue/s raised	TfNSW response
<b>Support for Proposed Activity</b>			
	BN001, BN002, BN003, BN004, BN005, BN006, BN007, BN008, BN009, BN010, BN011, BN012, BN013, BN014	Fourteen submissions expressed support for the Proposed Activity.	The respondent's support for the Proposed Activity is noted.
<b>1 Community and stakeholder consultation</b>			
1.1	BN015	One submission raised a query regarding whether an agreement had been made between TfNSW and the Bayside Council on the proposed Kingsgrove Avenue commuter car park and Bexley Road footpath works.	<p>Consultation with Bayside Council has commenced and further consultation would be undertaken as part of the ongoing development of the Proposed Activity during detailed design. Consultation would include discussion on pedestrian and vehicle movements, impacts to road pavements under Council's care and control, impact on Council operated footpaths and the impacts to the existing commuter car park area (largely located on Council land).</p> <p>The handover of new or altered assets for the operation and maintenance by Bayside Council would be formalised at the completion of the Proposed Activity.</p>
1.2	BN015	One submission raised a query regarding whether consultation with the authority responsible for the ethane pipeline had been undertaken.	<p>Consultation with the asset owner of the existing ethane gas pipeline passing through the Kingsgrove Avenue commuter car park has taken place during the initial concept stage to obtain input on the ability to construct structures around and above the service. Further consultation would occur during the detailed design of the Proposal by the nominated Construction Contractor.</p> <p>Approval of the applicable design detail, and construction methodology would be sought from the asset owner (APA Group) prior to the commencement of works in the station carpark area.</p>

No	Stakeholder	Issue/s raised	TfNSW response
<b>2 Construction activities</b>			
2.1	BN012	One submission suggested to stage the works to avoid disruption and station closures.	<p>Other than during scheduled rail shut down weekends, the station would remain open throughout the construction period. These periods are typically limited to a couple of days to minimise disruption.</p> <p>The construction methodology would be further developed during the detailed design of the Proposal by the Construction Contractor in consultation with TfNSW. Indicative construction staging is outlined in section 3.2 of the REF. Refinement of the proposed construction methodology would aim to minimise potential disruptions in line with the mitigation measures as identified in section 7.2 of the REF.</p> <p>TfNSW will regularly notify the community about construction activities and mitigation measures throughout the project.</p>
2.2	BN015	One submission raised a concern regarding the constructability of the design and how temporary access would be provided.	<p>Indicative construction staging, including enabling works, is outlined in section 3.2 of the REF. Access to the station would be provided throughout the whole of the construction period, including provision of a temporary access structure during construction of the lift and new stairway.</p> <p>The construction methodology, including temporary access arrangements would be further developed during the detailed design of the Proposal by the Construction Contractor in consultation with TfNSW.</p>
<b>3 Cumulative Impacts</b>			
3.1	BN015	One submission raised a concern regarding the cumulative impact of the construction of the Proposed Activity and a potential future development on Shaw Street.	<p>The Development Application for 16-20 Shaw Street is currently being assessed by Bayside Council and is therefore yet to approved. The proposal for 16-20 Shaw Street is not considered to be major development that would substantive cumulative impacts should the construction of the two projects occur concurrently.</p> <p>Temporary / short term closures of Shaw Lane may be required as part of the works (subject to development of the construction methodology by the Construction Contractor). Consultation would be undertaken with affected property owners / businesses in advance, including the applicant of this Development Application, to manage any impacts.</p>

No	Stakeholder	Issue/s raised	TfNSW response
<b>4</b>	<b>Scope of works</b>		
4.1	BN001	One submission requested that the design incorporates additional seating.	<p>The Proposed Activity does not currently propose to remove any existing seating on the station platforms. Due to the limited platform widths, space is constrained for additional seating at a number of locations. TfNSW will investigate options to reinstate any seating that is removed during detailed design.</p> <p>The upgrade of the commuter car park would also include the provision of seating.</p>
4.2	BN015	One submission requested that pathways to the bus stops located on Bexley Road, Shaw Street and Slade Road be upgraded to be compliant as part of the Proposed Activity.	<p>The Proposed Activity would include upgrading the existing footpaths to the north and south of the station entrance along the Bexley Road overbridge, including minor re-grading of footpaths and installation of landings.</p> <p>While the objectives of the Transport Access Program are focused on improving access to the station itself, the final extent of accessible pathways connecting to surrounding areas such as Bexley Road, Shaw Street and Slade Road would be confirmed during detailed design, in consultation with Bayside Council.</p>
4.3	BN011	One submission requested the upgrading of the footpath leading to Bexley Road with rain and wind protection.	<p>This request is beyond the scope of the Transport Access Program objectives which are focused on improving access to the station itself, primarily around achieving DSAPT and DDA compliant access.</p> <p>Requests for upgrades to the footpath leading to Bexley Road with rain and wind protection would be provided to Bayside Council for their consideration.</p>
4.4	BN015	One submission requested the design to extend the canopy coverage on the platform to meet existing standards.	<p>A new canopy would be constructed covering the stairs, adjacent platform areas, and the new lift access (platform and road levels). While there would be a gap between the new and existing canopies, this has been designed in order to preserve the heritage and urban design features of the station building in consultation with a range of stakeholders including Sydney Trains heritage specialists.</p>
4.5	BN006	One submission requested the design to remove the existing ramp and stairs to the commuter car park.	<p>The existing ramp and step would be removed and replaced with a new ramp to the commuter car park as part of the project scope.</p>

No	Stakeholder	Issue/s raised	TfNSW response
<b>5</b>	<b>Traffic and Transport</b>		
5.1	BN015	One submission requested the Proposed Activity to include the provision of kiss-and-ride facilities.	<p>There are no current kiss-and-ride facilities in the vicinity of the station.</p> <p>TfNSW would investigate the potential for formal kiss-and-ride facilities during detailed design, noting that the provision of a kiss-and-ride area may result in the loss of additional commuter car parking spaces to accommodate this facility.</p>
5.2	BN015	One submission requested the Proposed Activity include the provision of bicycle parking facilities.	<p>There are currently no bicycle parking facilities within the station precinct.</p> <p>The scope of the Transport Access Program objectives is focused on providing DSAPT and DDA compliant access to the station itself.</p> <p>However, TfNSW would consult further with Bayside Council during detailed design regarding the opportunity to include bicycle parking facilities within the layout for the upgraded Kingsgrove Avenue car parking area if space permits.</p>
5.3	BN016	One submission raised a concern regarding commuter car park availability during construction.	<p>The construction of the Proposed Activity would require the use of the existing commuter car park on Kingsgrove Avenue as a temporary construction compound prior to the proposed works at this location (subject to confirmation of the detailed construction methodology).</p> <p>The temporary closure of the existing commuter car park (to allow for use as a temporary construction compound and to allow for the construction works to occur at this location) would result in the temporary removal of 33 parking spaces during the construction period and the net loss of 11 parking spaces following construction.</p> <p>As discussed in section 6.1 of the REF, it is considered that the remaining available parking along Kingsgrove Avenue within a walking distance of approximately 400 metres would be sufficient to result in minor overall impacts to existing commuters.</p> <p>Light vehicle parking for construction vehicles is proposed to be within the site compound to the east of Bexley Road. Potential impacts to the un-timed street parking on Kingsgrove Avenue running parallel to the rail corridor and the secondary car park located to the south of the Proposal behind Bexley North Library are expected to be minor.</p> <p>TfNSW would also consult further with Bayside Council regarding the potential use of the existing Bexley North Library and Slade Road car parks for commuter parking, to compensate for the loss of parking during construction and net loss of 11 parking spaces following construction.</p>

No	Stakeholder	Issue/s raised	TfNSW response
5.4	BN007 and BN016	Two submissions requested that the Proposed Activity include the re-painting of lines in car park next to the Bexley North Library (BN007) and on Kingsgrove Avenue (BN016).	<p>TfNSW would consult further with Bayside Council regarding the use of the current Bexley North Library car park to assist with offsetting the loss of 11 parking spaces during operation.</p> <p>This would include activities such as changing the parking signs and re-painting of lines in the car park, if impacted by the construction activities.</p>
5.5	BN014, BN015 and BN016	<p>Three community submissions raised concerns regarding the number of parking spaces to be provided in the upgraded commuter car park.</p> <p>One of the submissions also highlighted that there is currently insufficient parking at the station due to commuters from outside the area and WestConnex M5 construction workers using the car park.</p> <p>This submission included a request for the time restrictions to be removed from the car park next to the Bexley North Library and the Slade Road car park, and for the upgraded commuter car park to include spaces for motorbikes.</p>	<p>The current commuter car park layout does not comply with Australian Standards as there is a lack of a turning bay and insufficient aisle width. The loss of parking spaces is due to the need to provide space for the new ramp access, vehicle turning space and shared zone for the accessible parking spaces.</p> <p>It is considered that the remaining available free and un-timed parking along Kingsgrove Avenue within a walking distance of approximately 400 metres is sufficient to compensate for the minor overall reduction in parking available for the station.</p> <p>TfNSW would consult further with Bayside Council regarding the potential use of the Bexley North Library car park and the council car park on Slade Road for commuter parking. This would include the potential to offset the proposed loss of commuter car spaces (temporary during construction and permanent during operation) within the Kingsgrove Avenue car park. Options to be considered would include modifying parking signage and/or re-painting of lines within the existing car park.</p> <p>With respect to issues regarding construction works and parking from the WestConnex M5 project, these can be directed to <a href="mailto:info@westconnex.com.au">info@westconnex.com.au</a>.</p>



No	Stakeholder	Issue/s raised	TfNSW response
7	<b>Urban design, landscape and visual amenity</b>		
7.1	BN013	One submission raised a concern regarding the visual impact of the design on Bexley Road and the historical character of the station.	<p>The visual impact of the lift would be reduced as far as practicable during detailed design through collaboration with the Contractor.</p> <p>The visual impact would also be reduced by the proposed simple form (encapsulating the lift core within the entrance structure) and material palette.</p> <p>Impacts to the heritage aspects of Bexley North Station such as the 1931 platform and platform building, would be minimised as much as possible as part of the development of the Proposed Activity.</p> <p>Additionally, the new canopy has been designed with a gap between the new and existing canopies in order to preserve the heritage and urban design features of the station building. This design had been developed in consultation with a range of stakeholders including Sydney Trains heritage specialists.</p> <p>The visual and heritage impacts of the proposed upgrades are discussed in Sections 6.2 and 6.5 of the REF. The Visual Impact Assessment and Heritage Assessment are provided in Technical Papers 1 and 3.</p>

### Other stakeholder submissions

Table 2.3 outlines issues raised by Bayside Council and the SES in their submissions, along with TfNSW's response. Both submissions were generally supportive of the upgrades, but raised considerations for construction and ongoing management of the station.

**Table 2.2: Response to other stakeholder submissions received**

No	Stakeholder	Issue/s raised	TfNSW response
1	<b>Biodiversity</b>		
1.1	Bayside Council	Raised that works should be considerate of nearby habitat and potential construction impacts on threatened species.	<p>Potential impacts to biodiversity and proposed mitigation measures are discussed in section 6.7 of the REF and Technical Paper 4.</p> <p>Construction would be undertaken in accordance with the TfNSW <i>Fauna Management Guideline</i> to avoid and/or minimise construction-related impacts on fauna and fauna habitat.</p>

No	Stakeholder	Issue/s raised	TfNSW response
1.2	Bayside Council	Included requirement that works are not to encroach on the Tree Protection Zone (TPZ) or Structural Root Zone (SRZ) of the trees to be retained.	Section 6.7.3 of the REF and Technical Paper 5 outline mitigation measures for potential impacts to trees to be retained. Tree protection would be undertaken in line with <i>AS 4970-2009 Protection of Trees on Development Sites</i> and would include exclusion fencing of TPZs. These measures would be implemented during the construction of the Proposed Activity.
1.3	Bayside Council	Included requirement that the offset replacement plantings should be referred to Bayside Council Infrastructure Assets, for suitable locations and species selection.	TfNSW would replace the trees removed as a result of the project in accordance with the TfNSW <i>Vegetation Offset Guide (2016)</i> . In accordance with Section 5 of the guideline, 32 trees would be required to meet the offset requirement. TfNSW would consult with Bayside Council and/or Sydney Trains for suitable locations for the offsets and their species.
<b>2</b>	<b>Community and stakeholder consultation</b>		
2.1	Bayside Council	Raised the importance of providing local residences and businesses with a detailed timeline of the works and potential traffic diversions.	<p>As discussed in Chapter 5 of the REF, should TfNSW determine to proceed with the Proposal the project team would keep the community, Council and other key stakeholders informed of the Project progress, expected impacts and mitigation measures.</p> <p>The interaction with the community would be undertaken in accordance with a Community Liaison Management Plan to be developed prior to the commencement of construction, including the distribution of regular notifications to the local community and key stakeholders such as Council about construction activities and mitigation measures.</p> <p>A Construction Traffic Management Plan would also be prepared as part of the CEMP to assist in informing the motorists and pedestrians of the works.</p>

No	Stakeholder	Issue/s raised	TfNSW response
<b>3</b>	<b>Hydrology and water quality</b>		
3.1	SES	<p>Comment that construction activities may increase flooding in the area.</p> <p>Request that the SES be notified of the status of construction works in the event of significant rainfall forecasts.</p>	<p>The construction methodology would be further developed during the detailed design of the Proposal by the Construction Contractor in consultation with TfNSW.</p> <p>The Construction Contractor would be required to notify the SES of the status of construction works if there are significant rainfall events forecast during construction.</p>
3.2	SES	<p>Suggestion to incorporate measures to minimise flood risk to the community through site design and stormwater management.</p>	<p>The current design has considered the potential impacts of flooding on the operation of the Proposal. Further investigations would be undertaken during the detailed design stage to ensure the design addresses potential flood risks to operational activities and equipment, and that the new infrastructure does not worsen the existing flooding situation</p>
3.3	SES	<p>Recommendation to ensure that people using the station are aware of the flood risk and that an appropriate business emergency plan be developed to assist in preparedness, response and recovery from flooding.</p>	<p>Sydney Trains will be responsible for operating Bexley North Station, including the lift and new works after completion. The station has a current Station Incident Management Plan (SIMP) which includes emergency evacuation procedures. Following completion of the Proposed Activity, TfNSW would handover the asset to Sydney Trains who would then make any necessary amendments to the Bexley North Station SIMP to reflect any alterations to operational requirements resulting from the use of the proposed station infrastructure.</p>

No	Stakeholder	Issue/s raised	TfNSW response
<b>4</b>	<b>Scope of works</b>		
4.1	Bayside Council	Request that the design incorporate renewal of the fencing on Bexley Road Overbridge (both rail and road-side of the footpath). This includes the removal of the guard rail and replacement with safe, softer urban street-scaping material with investigations into the inclusion of suitable greenery.	<p>Renewal of the fencing (anti-throw screens) on the overbridge is outside the scope of the Proposed Activity, however this request would be passed on to Sydney Trains for their consideration.</p> <p>Similarly, replacement of the guard rail on the road-side of the footpaths is also outside of the scope of the Proposed Activity, however the suggestion would be passed on to RMS for their consideration. The existing guard rail would be required to be retained due to the mandatory standards for protection on rail overbridges.</p> <p>With respect to the provision of urban street-scaping and inclusion of suitable greenery, TfNSW would offset the trees removed during the construction of the project as per the TfNSW <i>Vegetation Offset Guide</i>. As part of this offsetting, TfNSW would liaise with Council and/or Sydney Trains as to appropriate locations for re-planting, preferably near the station.</p> <p>Due to the restricted footpath width on Bexley Road, the inclusion of greenery at this location is not feasible.</p>
4.2	Bayside Council	Request that footpath works are to a 'share-path' standard in and around the station, designed for both pedestrians and bicycle users.	<p>The objective of the Transport Access Program is to achieve DSAPT and DDA compliant access at all NSW train stations, focusing on improving access to the station itself. As such, the provision of a shared path is not part of the overall objective of the Proposed Activity.</p> <p>It is also not considered that there is sufficient space for a shared path along Bexley Road footpaths at the location of the station and is not considered safe in the vicinity of the station entrance.</p> <p>Additionally, any connections to the bike paths are outside the scope of these upgrade works, however the suggestion has been passed onto the TfNSW Active Transport Integration team.</p>
4.3	Bayside Council	Request for the inclusion of quality, non-slip architectural-quality paving connecting both the town centre and the car parking areas to the station entrance.	The footpath upgrades would be completed to the required standards for public footpaths, such as the Austroads <i>Guide to Road Design – Part 6A: Pedestrian and Cyclist Paths</i> and would match the existing materials.

No	Stakeholder	Issue/s raised	TfNSW response
4.4	Bayside Council	Request for shared-zone and access ramp on Shaw Lane.	Shaw Lane is not considered to be a safe pedestrian environment and is therefore not included in the scope of works for the Proposal. Shaw Street should be used as the main access point to the Bexley North Library car park.
<b>5 Traffic and transport</b>			
5.1	Bayside Council	Request for the Proposed Activity to include the provision of kiss-and-ride facilities.	<p>There are no current kiss-and-ride facilities in the vicinity of the station.</p> <p>TfNSW would investigate the potential to provide formal kiss-and-ride facilities during detailed design, noting formal kiss-and-ride space(s) may require the loss of additional commuter car parks to accommodate this facility.</p>
5.2	Bayside Council	Request for the Proposed Activity to include the provision of bike parking facilities.	<p>There are no current bicycle parking facilities within the station precinct. The scope of the Transport Access Program objectives is focused on providing DSAPT and DDA compliant access to the station.</p> <p>TfNSW would consult further with Bayside Council regarding the opportunity to include bicycle parking facilities within the layout for the station surrounds if space permits.</p>
5.3	Bayside Council	Request to ensure that safe, accessible car park facilities remain in operation during the duration of the works.	<p>The construction of the Proposed Activity would require the use of the existing commuter car park on Kingsgrove Avenue as a construction compound. The use of the commuter car park would result in the temporary removal of 33 parking spaces during the construction period and the net loss of 11 parking spaces during operation.</p> <p>As discussed in section 6.1 of the REF, it is considered that the remaining available parking along Kingsgrove Avenue within a walking distance of approximately 400 metres would be sufficient to result in minor overall impacts to existing commuters.</p> <p>Light vehicle parking for construction vehicles is proposed to be within the site compound to the east of Bexley Road. Potential impacts to the un-timed street parking on Kingsgrove Avenue running parallel to the rail corridor and the secondary car park located to the south of the Proposal behind Bexley North Library are expected to be minor.</p> <p>TfNSW would also consult further with Bayside Council regarding the potential use of the existing Bexley North Library car park and Slade Road car park for commuter parking, to compensate for the loss of parking during construction.</p>

No	Stakeholder	Issue/s raised	TfNSW response
5.4	Bayside Council	Request that the loss of car parks in the commuter car park is offset by upgrading the Bexley North Library public car park.	The final layout of the Proposed Activity would seek to maximise the layout of the proposed Kingsgrove Avenue car park area in order to provide as many compliant car parking spaces as possible. TfNSW will also undertake further consultation with Bayside Council regarding the potential use of the Bexley North Library car park and Slade Road car park for commuter parking in an effort to offset the loss of the 11 spaces as part of the Proposed Activity (and temporary construction car park losses). This may include modifying the parking signage and re-painting of lines in the car park.

## 2.4 Future consultation

Should TfNSW proceed with the Proposed Activity, consultation activities would continue, including consultation with Sydney Trains, Bayside Council, RMS, utility authorities, and other relevant stakeholders regarding design development. In addition, TfNSW would notify residents, businesses and community members in the lead up to and during construction.

The consultation activities would help to ensure that:

- local council and other stakeholders have an opportunity to provide feedback on the detailed design
- the community and stakeholders are notified in advance of any upcoming works, including changes to pedestrian or traffic access arrangements and out of hours construction activities
- accurate and accessible information is made available
- a timely response is given to issues and concerns raised by the community
- feedback from the community is encouraged.

The [TfNSW email address](mailto:projects@transport.nsw.gov.au)<sup>3</sup>, TfNSW Infoline (1800 684 490) and 24-hour Construction Response Line 1800 775 465 would continue to be available during the construction phase. Targeted consultation methods, such as use of letters, notifications, signage and verbal communications, would continue to occur. The [TfNSW website](http://www.transport.nsw.gov.au/bexley-north)<sup>4</sup> would also include updates on the progress of construction.

<sup>3</sup> [projects@transport.nsw.gov.au](mailto:projects@transport.nsw.gov.au)

<sup>4</sup> <http://www.transport.nsw.gov.au/bexley-north>

## 3 Consideration of the environmental impacts

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### 3.1 *Environmental Planning and Assessment Act 1979*

The REF addresses the requirements of section 5.5 of the EP&A Act. In considering the Proposed Activity, all matters affecting or likely to affect the environment are addressed in the REF and the Determination Report and associated documentation.

In accordance with the checklist of matters pursuant to clause 228(3) of the EP&A Regulation, an assessment is provided in Chapter 6 of the REF and Appendix B of the REF.

In respect of the Proposed Activity an assessment has been carried out regarding potential impacts on critical habitat, threatened species, populations or ecological communities or their habitats, under section 5.7 of the EP&A Act.

The likely significance of the environmental impacts of the Proposed Activity has been assessed in accordance with the then NSW Department of Planning's 1995 best practice guideline *Is an EIS Required?*<sup>5</sup> It is concluded that the Proposed Activity is not likely to significantly affect the environment (including critical habitat) or threatened species, populations of ecological communities, or their habitats. Accordingly, an environmental impact statement under Part 5.2 of the EP&A Act is not required.

### 3.2 *Environment Protection and Biodiversity Conservation Act 1999*

As part of the consideration of the Proposed Activity, all matters of national environmental significance (NES) and any impacts on Commonwealth land for the purposes of the EPBC Act have been assessed. In relation to NES matters, this evaluation has been undertaken in accordance with Commonwealth Administrative Guidelines on determining whether an action has, will have, or is likely to have a significant impact. A summary of the evaluation is provided in Chapter 6 and Appendix A of the REF.

It is considered that the Proposed Activity described in the REF is not likely to have a significant impact on any Commonwealth land and is not likely to have a significant impact on any matters of NES.

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<sup>5</sup> Refer to the National Library of Australia's 'Trove' website  
<http://trove.nla.gov.au/work/7003034?selectedversion=NBD11474648>

## **4 Conditions of Approval**

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If approved, the Proposed Activity would proceed subject to the Conditions of Approval included at Appendix B.



## 5 Conclusion

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Having regard to the assessment in the REF and consideration of the submissions received, it can be concluded that the Proposed Activity is not likely to significantly affect the environment (including critical habitat) or threatened species, populations of ecological communities, or their habitats. Consequently, an environmental impact statement is not required to be prepared under Part 5.2 of the EP&A Act.

It is also considered that the Proposed Activity does not trigger any approvals under Part 3 of the EPBC Act.

The Proposed Activity is expected to provide a better experience for public transport by improving interchange facilities and ensuring that Bexley North Station is accessible to customers with a disability, limited mobility, parents/carers with prams and customers with luggage.

The environmental impact assessment (REF and Determination Report) is recommended to be approved subject to the proposed mitigation and environmental management measures included in the Conditions of Approval (refer Appendix B).

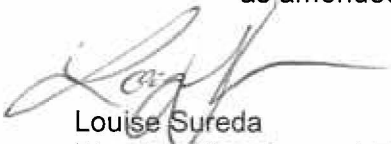
# Determination

## BEXLEY NORTH STATION UPGRADE

### APPROVAL

I, LOUISE SUREDA, as delegate of the Secretary, Transport for NSW:

1. Have examined and considered the Proposed Activity in the *Bexley North Station Review of Environmental Factors* (February 2019) and the *Bexley North Station Upgrade Determination Report* in (April 2019) in accordance with section 5.5 of the *Environmental Planning and Assessment Act 1979*.
2. Determine on behalf of Transport for NSW (the Proponent) that the Proposed Activity may be carried out in accordance with the Conditions of Approval in this Determination Report (April 2019), consistent with the Proposal described in the *Bexley North Station Upgrade Review of Environmental Factors* (February 2019) as amended by this Determination Report (April 2019).



Louise Sureda  
Director, Planning and Environment  
Infrastructure and Place  
Transport for NSW

Date: 2.4.19

## References

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TfNSW (February 2019), *Transport Access Program Bexley North Station Upgrade: Review of Environmental Factors*, Sydney

NSW Department of Planning (1995), *Is an EIS required?*, Sydney

TfNSW (November 2018), *More trains, More services*,  
<https://www.transport.nsw.gov.au/projects/more-trains-more-services>

# Appendix A      Review of Environmental Factors

Please refer to the TfNSW website to access the Bexley North Station Upgrade REF:

- *Bexley North Station Upgrade Review of Environmental Factors – Transport Access Program* (Desksite reference 6218044), February, 2019
- Link to Transport for NSW website: <https://www.transport.nsw.gov.au/projects/current-projects/bexley-north-station-upgrade>

# Appendix B Conditions of Approval

## CONDITIONS OF APPROVAL

### Bexley North Station Upgrade

Note: these conditions of approval must be read in conjunction with the final mitigation measures in the Bexley North Station Upgrade Review of Environmental Factors.

#### Schedule of acronyms and definitions used:

Acronym	Definition
ADEIA	Associate Director Environmental Impact Assessment (or nominated delegate)
ADEM	Associate Director Environmental Management (or nominated delegate)
ADSPD	Associate Director, Sustainability, Planning & Development (or nominated delegate)
CECR	Construction Environmental Compliance Report
CEMP	Construction Environmental Management Plan
CLMP	Community Liaison Management Plan
CoA	Conditions of Approval
dBA	Decibels (A-weighted scale)
ECM	Environmental Controls Map
EIA	Environmental Impact Assessment
EMR	Environmental Management Representative
EMS	Environmental Management System
EPA	NSW Environment Protection Authority
EP&A Act	<i>Environmental Planning and Assessment Act 1979</i>
EPL	Environment Protection Licence issued by the Environmental Protection Authority under the <i>Protection of the Environment Operations Act 1997</i> .
ISO	International Standards Organisation
OEH	NSW Office of Environment and Heritage
OOHWP	Out of Hours Works Protocol
PCSR	Pre Construction Sustainability Report
PDP	Public Domain Plan
PECM	Pre-construction environmental compliance matrix
POCR	Pre-operational compliance report

<b>Acronym</b>	<b>Definition</b>
<b>RBL</b>	Rating Background Level
<b>REF</b>	Review of Environmental Factors
<b>TfNSW</b>	Transport for NSW
<b>TMP</b>	Traffic Management Plan
<b>UDP</b>	Urban Design Plan

<b>Term</b>	<b>Definition</b>
<b>Construction</b>	Includes all work in respect of the Project, other than survey, acquisitions, fencing, investigative drilling or excavation, building/road dilapidation surveys, or other activities determined by the TfNSW ADEM to have minimal environmental impact such as minor access roads, minor adjustments to services/utilities, establishing temporary construction compounds (in accordance with this approval), or minor clearing (except where threatened species, populations or ecological communities would be affected, unless otherwise agreed by the ADEM).
<b>Contamination</b>	The presence in, on or under land of a substance at a concentration above the concentration at which the substance is normally present in, on or under (respectively) land in the same locality, being a presence that presents a risk of harm to human health or any other aspect of the environment.
<b>Designated Works</b>	Includes tunnelling, blasting, piling, excavation or bulk fill or any vibratory impact works including jack hammering and compaction, for Construction.
<b>Emergency Work</b>	Includes works to avoid loss of life, damage to external property, utilities and infrastructure, prevent immediate harm to the environment, contamination of land or damage to a heritage (indigenous or non-indigenous) item.
<b>Environmental Impact Assessment (EIA)</b>	The documents listed in Condition 1 of this approval.
<b>Environmental Management Representative</b>	An independent environmental representative appointed to the Project or a delegate nominated by Transport for NSW.
<b>Noise Sensitive Receiver</b>	In addition to residential dwellings, noise sensitive receivers include, but are not limited to, hotels, entertainment venues, pre-schools and day care facilities, educational institutions (e.g. schools, TAFE colleges), health care facilities (e.g. nursing homes, hospitals), recording studios, places of worship/religious facilities (e.g. churches), and other noise sensitive receivers identified in the environmental impact assessment.
<b>Reasonable and feasible</b>	Consideration of best practice taking into account the benefit of proposed measures and their technological and associated operational application in the NSW and Australian context. Feasible relates to engineering considerations and what is practical to build. Reasonable relates to the application of judgement in arriving at a decision, taking into account: mitigation benefits, cost of mitigation versus benefits provided, community views and nature and extent of potential improvements.
<b>the Project</b>	The construction and operation of the Bexley North Station Upgrade as described in the Environmental Impact Assessment.
<b>the Proponent</b>	A person or body proposing to carry out an activity under Division 5.1 of the EP&A Act – in the case of the Project, Transport for NSW.

CoA number	Condition
<b>General</b>	
<b>1</b>	<p><b>Terms of Approval</b></p> <p>The Project shall be carried out generally in accordance with the environmental impact assessment (EIA) for this Project, which comprises the following documents:</p> <ul style="list-style-type: none"> <li>a) <i>Bexley North Station Upgrade – Review of Environmental Factors</i>, (WSP, February 2019)</li> <li>b) <i>Bexley North Station Upgrade – Determination Report</i>, (WSP, March 2019)</li> </ul> <p>In the event of an inconsistency between these conditions and the EIA, these conditions will prevail to the extent of the inconsistency.</p>
<b>2</b>	<p><b>Project Modifications</b></p> <p>Any modification to the Project as approved in the EIA would be subject to further assessment. This assessment would need to demonstrate that any environmental impacts resulting from the modifications have been minimised. The assessment shall be subject to approval under delegated authority by TfNSW. The Proponent shall comply with any additional requirements from the assessment of the Project modification.</p>
<b>3</b>	<p><b>Statutory Requirements</b></p> <p>These conditions do not relieve the Proponent of the obligation to obtain all other licences, permits, approvals and land owner consents from all relevant authorities and land owners as required under any other legislation for the Project. The Proponent shall comply with the terms and conditions of such licences, permits, approvals and permissions.</p>
<b>4</b>	<p><b>Pre-Construction Environmental Compliance Matrix</b></p> <p>A Pre-Construction Environmental Compliance Matrix (PECM) for the Project (or such stages of the Project as agreed to by the Associate Director Environmental Management (ADEM)) shall be prepared detailing compliance with all relevant conditions and mitigation measures prior to commencement of construction. The PECM shall also include details of approvals, licences and permits required to be obtained under any other legislation for the Project.</p> <p>A copy of the PECM shall be submitted to the ADEM for approval, at least 21 days prior to commencement of construction of the Project (or within such time as otherwise agreed to by the ADEM).</p>
<b>5</b>	<p><b>Construction Environmental Compliance Report</b></p> <p>The Proponent shall prepare a construction environmental compliance report (CECR) which addresses the following matters:</p> <ul style="list-style-type: none"> <li>(a) compliance with the construction environmental management plan (CEMP) and these conditions</li> <li>(b) compliance with Infrastructure Sustainability Council of Australia (ISCA) Infrastructure Sustainability Rating Scheme (v1.2)</li> <li>(c) compliance with any approvals or licences issued by relevant authorities for construction of the Project</li> <li>(d) implementation and effectiveness of environmental controls (the assessment of effectiveness should be based on a comparison of actual impacts against performance criteria identified in the CEMP)</li> <li>(e) environmental monitoring results, presented as a results summary and analysis</li> <li>(f) details of the percentage of waste diverted from landfill and the percentage of spoil beneficially reused</li> </ul>



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- (g) number and details of any complaints, including summary of main areas of complaint, actions taken, responses given and intended strategies to reduce recurring complaints (subject to privacy protection)
- (h) details of any review and amendments to the CEMP resulting from construction during the reporting period
- (i) any other matter as requested by the ADEM.

The Proponent shall:

- i) submit a copy of the CECR to the EMR for review. The EMR is to be given a minimum period of 7 days to review and provide any comments to the Proponent in relation to the CECR
- ii) submit a copy of the CECR to the ADEM (or nominated delegate) for approval upon completion of the EMR review period.

The first CECR shall report on the first six months of construction and be submitted within 21 days of expiry of that period (or at any other time interval agreed to by the ADEM). CECRs shall be submitted no later than six months after the date of submission of the preceding CECR (or at other such periods as requested by the ADEM) for the duration of construction.

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**6      Pre-Operation Compliance Report**

A pre-operation compliance report (POCR) for the Project shall be prepared, prior to commencement of operation of the Project. The POCR shall detail compliance with all conditions of approval, licences and permits required to be obtained under any other legislation for the project.

The Proponent shall:

- (a) submit a copy of the POCR to the EMR for review. The EMR is to be given a minimum period of 7 days to review and provide any comments to the Proponent in relation to the POCR.
- (b) upon completion of the EMR review period submit a copy of the POCR to the ADEM (or nominated delegate) for approval. The POCR is to be provided to the ADEM at least one month prior to the scheduled operation of the Project (or such time as otherwise agreed to by the ADEM).

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**7      Graffiti and advertising**

Hoardings, site sheds, fencing, acoustic walls around the perimeter of the site, and any structures within the project footprint or built as part of the Project are to be maintained free of graffiti and advertising not authorised by the Proponent during the construction period. Graffiti and unauthorised advertising will be removed or covered within the following timeframes:

- (a) offensive graffiti will be removed or concealed within 24 hours
  - (b) highly visible (yet inoffensive) graffiti will be removed or concealed within a week
  - (c) graffiti that is neither offensive or highly visible will be removed or concealed within a month
  - (d) any unauthorised advertising material will be removed or concealed within 24 hours.
-

## Communications

### 8 Community Liaison Management Plan

A Community Liaison Management Plan (CLMP) shall be prepared and implemented to engage with government agencies, relevant councils, landowners, community members and other relevant stakeholders (such as utility and service providers, bus companies, Taxi Council and businesses). The CLMP shall comply with the obligations of these conditions and should include, but not necessarily be limited to:

- a) details of the protocols and procedures for disseminating information and liaising with the community and other key stakeholders about construction activities (including timing and staging) and any associated impacts during the construction period
- b) stakeholder and issues identification and analysis
- c) procedures for dealing with complaints or disputes and response requirements, including advertising the 24-hour construction response line number
- d) details (including a program) of training for all employees, contractors and sub-contractors on the requirements of the CLMP.

The CLMP shall be prepared to the satisfaction of the Director Community Engagement (or nominated delegate) prior to the commencement of construction and implemented, reviewed and revised as appropriate during construction of the Project.

### 9 Community Notification and Liaison

The local community shall be advised of any activities related to the Project with the potential to impact upon them.

Prior to any site activities commencing and throughout the Project duration, the community is to be notified of works to be undertaken, the estimated hours of construction and details of how further information can be obtained (i.e. contact telephone number/email, website, newsletters etc.) including the 24-hour construction response line number.

Construction-specific impacts including information on traffic changes, access changes, detours, services disruptions, public transport changes, high noise generating work activities and work required outside the nominated working hours shall be advised to the local community at least seven days prior to such works being undertaken or other period as agreed to by the Director Community Engagement or as required by the Environment Protection Authority (EPA) (where an Environment Protection Licence (EPL) is in effect).

### 10 Website

The Proponent shall provide electronic information (or details of where hard copies of this information may be accessed by members of the public) related to the Project, on dedicated pages within its existing website, including:

- a) a copy of the documents referred to under Condition 1 of this approval
- b) a list of environmental management reports that are publicly available
- c) 24-hour contact telephone number for information and complaints.

All documents uploaded to the website must be compliant with the Web Content Accessibility Guidelines 2.0.

**11 Complaints Management**

The Proponent shall provide a 24-hour construction response line number.

Details of all complaints received during construction are to be recorded on a complaints register. A verbal response to phone enquiries on what action is proposed to be undertaken is to be provided to the complainant within two hours during all times construction is being undertaken and within 24 hours during non-construction times (unless the complainant agrees otherwise). A verbal response to written complaints (email/letter) should be provided within 48 hours of receipt of the communication. A detailed written response is to be provided to the complainant within seven calendar days for verbal and/or written complaints.

Information on all complaints received during the previous 24 hours shall be forwarded to the TfNSW Community Engagement Manager and the TfNSW Environment and Planning Manager by 3pm each working day.

**Environmental Management**

**12 Construction Environmental Management Plan**

A Construction Environmental Management Plan (CEMP) shall be prepared prior to commencement of construction which addresses the following matters, as a minimum:

- a) traffic and pedestrian management (in consultation with the relevant roads authority)
- b) noise and vibration management
- c) water and soil management
- d) air quality management (including dust suppression)
- e) indigenous and non-indigenous heritage management
- f) flora and fauna management
- g) storage and use of hazardous materials
- h) contaminated land management (including acid sulphate soils)
- i) weed management
- j) waste management
- k) sustainability
- l) environmental incident reporting and management procedures
- m) non-compliance and corrective/preventative action procedures.

The CEMP shall:

- i) comply with the Conditions of Approval, conditions of any licences, permits or other approvals issued by government authorities for the Project, all relevant legislation and regulations, and accepted best practice management
- ii) comply with the relevant requirements of *Guideline for Preparation of Environmental Management Plans* (Department of Infrastructure, Planning and Natural Resources, 2004)
- iii) include an Environmental Policy.

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(cont.)

The Proponent shall:

1. consult with government agencies and relevant service/utility providers as part of the preparation of the CEMP
2. submit a copy of the CEMP to EMR for review
3. submit a copy of the CEMP to the ADEM (or nominated delegate) for approval
4. review and update the CEMP at regular intervals, and in response to any actions identified as part of the EMR's audit of the document
5. ensure updates to the CEMP are made within seven days of the completion of the review or receipt of actions identified by any audit of the document, and be submitted to the ADEM for approval.

The CEMP must be approved by the ADEM prior to the commencement of construction work associated with the Project.

13

**Environment Personnel**

Suitably qualified and experienced environmental management personnel shall be available and be responsible for implementing the environmental objectives for the Project, including undertaking regular site inspections, preparation of environmental documentation and ensuring the Project meets the requirements of the Environmental Management System (EMS).

Details of the environmental personnel, including relevant experience, defined responsibilities and resource allocation throughout the project (including time to be spent on-site/off-site) are to be submitted for the approval of the ADEM, at least 21 days prior to commencement of construction of the Project (or within such time as otherwise agreed to by the ADEM).

Any adjustments to environmental resource allocations (on-site or off-site) are to be approved by the ADEM.

14

**Environmental Management Representative**

Prior to the commencement of construction, the ADEM shall appoint an EMR for the duration of the construction period for the Project.

The EMR shall provide advice to the ADEM in relation to the environmental compliance and performance of the Project. The EMR shall have responsibility for:

- (a) considering and advising the Proponent on matters specified in these conditions and compliance with such
- (b) reviewing and where required by the ADEM, providing advice on the Project's induction and training program for all persons involved in the construction activities and monitoring implementation
- (c) periodically auditing the Project's environmental activities to evaluate the implementation, effectiveness and level of compliance of on-site construction activities with authority approvals and licences, the CEMP and associated plans and procedures, including carrying out site inspections weekly, or as required by the ADEM
- (d) reporting weekly to the Proponent, or as required by the ADEM
- (e) issuing a recommendation to the Proponent for work to stop immediately, if in the view of the EMR circumstances so require. The stop work recommendation may be limited to specific activities if the EMR can easily identify those activities
- (f) requiring reasonable steps to be taken to avoid or minimise unintended or adverse environmental impacts
- (g) reviewing corrective and preventative actions to ensure the implementation of recommendations made from the audits and site inspections

CoA number	Condition
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- (h) providing reports to the Proponent on matters relevant to the carrying out of the EMR role as necessary
- (i) where required by the ADEM, providing advice on the content and implementation of the CEMP and environmental controls map (ECM) in accordance with the conditions
- (j) reviewing and approving updates to the CEMP.

The EMR shall be available during construction activities to inspect the site(s) and be present on-site as required.

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**15 Environmental Controls Map**

The Proponent shall prepare an environmental controls map (ECM) in accordance with TfNSW's *Guide to Environmental Controls Map (3TP-SD-015)* prior to the commencement of construction for implementation for the duration of construction. The ECM is to be endorsed by the EMR and may be prepared in stages as set out in the CEMP.

The Proponent shall submit a copy of the ECM to the EMR for review and endorsement. The EMR is to be given a minimum period of 7 days to review and endorse the ECM. Following receipt of the EMR's endorsement, the ECM shall be submitted to the ADEM (or nominated delegate) for approval, at least 14 days prior to commencement of construction (or such time as is otherwise agreed to by the ADEM).

The ECM shall be prepared as a map – suitably enlarged (e.g. A3 size or larger) for mounting on the wall of a site office and included in site inductions, supported by relevant written information.

Updates to the ECM shall be made within 7 days of the completion of the review or receipt of actions identified by any EMR audit of the document, and be submitted to the EMR for approval.

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**Hours of Work**

**16 Standard Construction Hours**

Construction activities shall be restricted to the hours of 7.00am to 6.00pm (Monday to Friday); 8.00am to 1.00pm (Saturday) and at no time on Sundays and public holidays except for the following works which are permitted outside these standard hours:

- a) any works which do not cause noise emissions to be more than 5 dBA higher than the rating background level (RBL) at any nearby residential property and/or other noise sensitive receivers
- b) out of hours work identified and assessed in the EIA or the approved Out of Hours Work Protocol (OOHWP)
- c) the delivery of plant, equipment and materials which is required outside these hours as requested by police or other authorities for safety reasons and with suitable notification to the community as agreed by the ADEM
- d) Emergency Work to avoid the loss of lives, property and/or to prevent environmental harm
- e) any other work as agreed by the ADEM (or nominated delegate) and considered essential to the Project, or as approved by the EPA (where an EPL is in effect).

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**17 High Noise Generating Activities**

Rock breaking or hammering, jack hammering, pile driving, vibratory rolling, cutting of pavement, concrete or steel and any other activities which result in impulsive or tonal noise generation shall not be undertaken for more than three hours, without a minimum one hour respite period unless otherwise agreed to by the ADEM, or as approved by the EPA (where relevant to the issuing of an EPL), unless inaudible at nearby residential properties and/or other noise sensitive receivers.

**Noise and Vibration**

**18      Construction Noise and Vibration**

Construction noise and vibration mitigation measures shall be implemented through the CEMP, in accordance with TfNSW's *Construction Noise and Vibration Strategy (7TP-ST-157)* and the EPA's *Interim Construction Noise Guideline* (Department of Environment and Climate Change, 2009). The mitigation measures shall include, but not be limited to:

- a) details of construction activities and an indicative schedule for construction works
- b) identification of construction activities that have the potential to generate noise and/or vibration impacts on surrounding land uses, particularly sensitive noise receivers
- c) detail what reasonable and feasible actions and measures shall be implemented to minimise noise impacts (including those identified in the EIA)
- d) procedures for notifying sensitive receivers of construction activities that are likely to affect their noise and vibration amenity, as well as procedures for dealing with and responding to noise complaints
- e) an Out Of Hours Work Protocol (OOHWP) for the assessment, management and approval of works outside the standard construction hours identified in Condition 10 of this approval, including a risk assessment process which deems the out of hours activities to be of low, medium or high environmental risk, is to be developed. All out of hours works are subject to approval by the ADEM, or as approved by the EPA (where relevant to the issuing of an EPL). The OOHWP should be consistent with TfNSW's *Construction Noise and Vibration Strategy (7TP-ST-157)*
- f) a description of how the effectiveness of actions and measures shall be monitored during the proposed works, clearly indicating the frequency of monitoring, the locations at which monitoring shall take place, recording and reporting of monitoring results and if any exceedance is detected, the manner in which any non-compliance shall be rectified.

**19      Vibration Criteria**

Vibration (other than from blasting) resulting from construction and received at any structure outside of the Project shall be limited to:

- a) for structural damage vibration – German Standard DIN 4150:Part 3 – 1999: *Structural Vibration in Buildings: Effects on Structures*
- b) for human exposure to vibration – the acceptable vibration values set out in the *Environmental Noise Management Assessing Vibration: A Technical Guideline* (Department of Environment and Conservation, 2006).

These limits apply unless otherwise approved by the ADEM through the CEMP.

**20      Piling**

Wherever practical, piling activities shall be completed using non-percussive piles. If percussive piles are proposed to be used, approval of the ADEM shall be obtained prior to commencement of piling activities.

**21      Non-Tonal Reversing Beepers**

Non-tonal reversing beepers (or an equivalent mechanism) shall be fitted and used on all construction vehicles and mobile plant regularly used on site (i.e. greater than one day) and for any out of hours work.

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22	<b>Property condition surveys</b>
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Subject to landowner agreement, property condition surveys shall be completed prior to piling, excavation or bulk fill or any vibratory impact works including jack hammering and compaction (Designated Works) in the vicinity of the following buildings/structures:

- (a) all buildings/structures/roads within a plan distance of 50 metres from the edge of the Designated Works
- (b) all heritage listed buildings and other sensitive structures within 100 metres from the edge of the Designated Works.

Property condition surveys need not be undertaken if a risk assessment indicates that selected buildings/structures/roads identified in (a) and (b) will not be affected as determined by a qualified geotechnical and construction engineering expert with appropriate registration on the National Professional Engineers Register prior to commencement of Designated Works.

Selected potentially sensitive buildings and/or structures shall first be surveyed prior to the commencement of the Designated Works and again immediately upon completion of the Designated Works.

All owners of assets to be surveyed, as defined above, are to be advised (at least 14 days prior to the first survey) of the scope and methodology of the survey, and the process for making a claim regarding property damage.

A copy of the survey(s) shall be given to each owner. A register of all properties surveyed shall be maintained.

Any damage to buildings, structures, lawns, trees, sheds, gardens, etc. as a result of construction activity direct and indirect (i.e. including vibration and groundwater changes) shall be rectified at no cost to the owner(s).

### Flora and Fauna

23	<b>Replanting program</b>
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All cleared vegetation shall be offset in accordance with TfNSW's *Vegetation Offset Guide* (9TP-SD-087). All vegetation planted on-site is to consist of locally endemic native species, unless otherwise agreed by the ADEM, following consultation with the relevant council, where relevant, and/or the owner of the land upon which the vegetation is to be planted.

24	<b>Removal of trees or vegetation</b>
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Separate approval, in accordance with TfNSW's *Application for Removal or Trimming of Vegetation* (9TP-SD-078), is required for the trimming, cutting, pruning or removal of trees or vegetation where the impact has not already been identified in the EIA for the Project. The trimming, cutting, pruning or removal of trees or vegetation shall be undertaken in accordance with the conditions of that approval.

**Contamination and Hazardous Materials**

**25      Duty to Notify**

If previously unidentified contamination is identified within the site, the Proponent is to determine whether there is a Duty to Report under section 60 of the *Contaminated Land Management Act 1997*, and notify the EPA in accordance with the EPA's Guidelines on the Duty to Report Contamination under the *Contaminated Land Management Act 1997* (Department of Environment and Climate Change, 2009).

**26      Unidentified Contamination (other than asbestos)**

If previously unidentified contamination (excluding asbestos) is discovered during construction, work in the affected area must cease immediately, and an investigation must be undertaken and a report prepared to determine the nature, extent and degree of any contamination. The level of reporting must be appropriate for the identified contamination in accordance relevant EPA guidelines, including *Guidelines for Consultants Reporting on Contaminated Sites* (OEH, 2011).

The proponent shall:

- (a) submit a copy of any contamination report to the EMR for review. The EMR is to be given a minimum period of seven days to review and provide any comments to the Proponent in relation to the report.
- (b) submit a copy of the report to the ADEM for consideration upon completion of the EMR review period. The ADEM shall determine whether consultation with the relevant council and/or EPA is required prior to continuation of construction works within the affected area.

**Note:** *In circumstances where both previously unidentified asbestos contamination and other contamination are discovered within a common area, nothing in these conditions shall prevent the preparation of a single investigation report to satisfy the requirements of both Condition 26 and Condition 27.*

**27      Asbestos Management**

If previously unidentified asbestos contamination is discovered during construction, work in the affected area must cease immediately, and an investigation must be undertaken and a report prepared to determine the nature, extent and degree of the asbestos contamination. The level of reporting must be appropriate for the identified contamination in accordance with relevant EPA and WorkCover guidelines and include the proposed methodology for the remediation of the asbestos contamination. Remediation activities must not take place until receipt of the investigation report.

Works may only recommence upon receipt of a validation report from a suitably qualified contamination specialist that the remediation activities have been undertaken in accordance with the investigation report and remediation methodology.

**Note:** *In circumstances where both previously unidentified asbestos contamination and other contamination are discovered within a common area, nothing in these conditions shall prevent the preparation of a single investigation report to satisfy the requirements of both Condition 26 and Condition 27.*



**28 Storage and Use of Hazardous Materials**

Construction hazard and risk issues associated with the use and storage of hazardous materials shall be addressed through risk management measures, which shall be developed prior to construction as part of the overall CEMP, in accordance with relevant EPA guidelines, TfNSW's *Chemical Storage and Spill Response Guidelines* (9TP-SD-066) and Australian and ISO standards. These measures shall include:

- a) the storage of hazardous materials, and refuelling/maintenance of construction plant and equipment to be undertaken in clearly marked designated areas that are designed to contain spills and leaks
- b) spill kits, appropriate for the type and volume of hazardous materials stored or in use, to be readily available and accessible to construction workers. Kits are to be kept at hazardous materials storage locations, in site compounds and on specific construction vehicles. Where a spill to a watercourse is identified as a risk, spill kits are to be kept in close proximity to potential discharge points in support of preventative controls
- c) all hazardous materials spills and leaks to be reported to site managers and actions to be immediately taken to remedy spills and leaks
- d) training in the use of spill kits to be given to all personnel involved in the storage, distribution or use of hazardous materials.

**29 Contamination investigation**

If recommended by the Phase 1 preliminary site investigation report, a Phase 2 detailed site investigation shall be undertaken prior to construction commencing. The assessment shall generally be undertaken in accordance with:

- (a) *The National Environment Protection (Assessment of Site Contamination) Amendment Measure (NEPM) 2013*
- (b) *Contaminated Sites - Sampling Design Guidelines (EPA, 1995)*
- (c) *AS4482 (2005) Guide to investigation and sampling of site with potentially contaminated soil (2005).*

The report shall be prepared in accordance with the DECCW's *Guidelines for Consultants Reporting on Contaminated Sites* (Office of Environment and Heritage, 2011). The report shall include a preliminary waste classification in accordance with the NSW EPA *Waste Classification Guidelines* (EPA, 2014).

Specific requirements for further investigation, remediation or management of any contamination within the identified areas recommended in the Stage 2 Detailed Site Investigation shall be included in the CEMP as appropriate.

If contamination is identified within the Site, the Proponent is to determine whether there is a Duty to Report under section 60 of the *Contaminated Land Management Act 1997* and the OEH Guidelines.

**30 Contamination Management Plan**

Specific requirements for further investigation, remediation and management of any potential contamination within the identified areas recommended in the Phase 2 contamination assessment shall be included in a contamination management plan (CMP) as appropriate.

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### Erosion and Sediment Control

**31 Erosion and Sediment Control**

Soil and water management measures shall be prepared and implemented as part of the CEMP for the mitigation of water quality and hydrology impacts during construction of the Project. The management measures shall be prepared in accordance with *Managing Urban Stormwater: Soils and Construction - Volume 1*, 4th Edition (Landcom, 2004).

### Lighting

**32 Lighting scheme**

A lighting scheme for the construction and operation of the Project is to be developed and approved by a suitably qualified lighting designer and prepared in accordance with AS 1158 "Road Lighting" and AS 4282 "Control of the Obtrusive Effect of Outdoor Lighting". The lighting scheme shall address the following as relevant:

- (a) consideration of lighting demands of different areas
- (b) strategic placement of lighting fixtures to maximise ground coverage
- (c) use of LED lighting
- (d) minimising light spill by directing lighting into the station and platform
- (e) control systems for lighting that dim or switch-off lights settings according to the amount of daylight the zone is receiving
- (f) motion sensors to control low traffic areas
- (g) allowing the lighting system to use low light or switch off light settings while meeting relevant lighting Standards requirements, and
- (h) ensuring security and warning lighting is not directed at neighbouring properties.

The proposed lighting scheme is to be submitted to and accepted by the TfNSW Urban Design Team.

### Sustainability

**33 Sustainability officer**

The Proponent shall appoint a suitably qualified and experienced sustainability officer who is responsible for implementing sustainability objectives for the Project, unless otherwise agreed with the ADSPD prior to the commencement of construction.

Details of the sustainability officer, including defined responsibilities, duration and resource allocation throughout the appointment consistent with the Proponent's sustainability objectives are to be submitted to the satisfaction of the ADSPD prior to preparation of the PCSR.

**34 Pre-construction sustainability report**

Prior to commencement of construction, a pre-construction sustainability report (PCSR) shall be prepared to the satisfaction of the ADSPD. The Report shall include the following minimum components:

- (a) a completed electronic checklist demonstrating compliance with ISCA Sustainability Infrastructure Rating Scheme (v1.2)
- (b) a statement outlining the Proponent's own corporate sustainability obligations, goals, targets, in house tools, etc.

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34 (cont.)	<p>(c) a section specifying a process to identify and progress innovation initiatives on the project as appropriate. The process should identify any areas of innovation that are currently being explored and/or implemented on the Project.</p> <p>The Proponent shall submit a copy of the PCSR to the ADSPD for approval, at least 14 days prior to the commencement of construction (or within such time as otherwise agreed to by the ADSPD).</p>
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**Urban design and landscaping**

35	<p><b>Urban Design Plan</b></p> <p>An Urban Design Plan (UDP) is to be submitted to TfNSW and endorsed by the Precincts and Urban Design team during detailed design. The UDP is to address the fundamental design principles as outlined in <i>'Around the Tracks' – urban design for heavy and light rail, TfNSW, Interim 2016</i>. The UDP shall:</p>
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- a) Demonstrate a robust understanding of the site through a comprehensive site analysis to inform the design direction, demonstrate connectivity with street networks, transport modes, active transport options, and pedestrian distances
- b) Identify opportunities and challenges
- c) Establish site specific principles to guide and test design options
- d) Demonstrate how the preferred design option responds to the design principles established in *'Around the Tracks'*, including consideration of Crime Prevention through Environmental Design Principles
- e) Demonstrate how the design has been developed to resolve the scale of the concourse entry structure and to unify the component parts of the station entrance. The scale of the proposed concourse roof is dictated by concealing the height of the lift shaft.

The UDP is to include the Public Domain Plan (PDP) for the chosen option and will provide analysis of the:

- a) Landscape design approach including design of pedestrian and bicycle pathways, street furniture, interchange facilities, new planting and opportunities for public art
- b) Materials Schedule including materials and finishes for proposed built works, colour schemes, paving and lighting types for public domain, fencing and landscaping
- c) An Artist's Impression or Photomontage to communicate the proposed changes to the precinct.

The following design guidelines are available to assist and inform the UDP for the Proposal:

1. TAP Urban Design Plan, Guidelines, TfNSW, Draft 2018
2. Commuter Car Parks, urban design guidelines, TfNSW, Interim 2017
3. Managing Heritage Issues in Rail Projects Guidelines, TfNSW, Interim 2016
4. Creativity Guidelines for Transport Systems, TfNSW, Interim 2016
5. Water Sensitive Urban Design Guidelines for TfNSW Projects, 2016

**Traffic and Access**

36	<p><b>Traffic Management Plan</b></p> <p>Prior to the commencement of construction, or as agreed with the ADEM, the Proponent shall prepare a construction traffic management plan (TMP) as part of the CEMP which addresses, as a minimum, the following:</p> <ul style="list-style-type: none"> <li>(a) ensuring adequate road signage at construction work sites to inform motorists and pedestrians of the work site ahead to ensure that the risk of road accidents and disruption to surrounding land uses is minimised</li> </ul>
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36 (cont.)	<ul style="list-style-type: none"> <li>(b) maximising safety and accessibility for pedestrians and cyclists</li> <li>(c) ensuring adequate sight lines to allow for safe entry and exit from the site</li> <li>(d) ensuring access to railway stations, businesses, entertainment premises and residential properties (unless affected property owners have been consulted and appropriate alternative arrangements made)</li> <li>(e) managing impacts and changes to on and off street parking and requirements for any temporary replacement provision</li> <li>(f) parking locations for construction workers away from stations and busy residential areas and details of how this will be monitored for compliance</li> <li>(g) liaise with Bayside Council to consider opportunities for nearby temporary commuter parking to offset those unavailable during construction e.g. Bexley North Library and Slade Road car parks.</li> <li>(h) liaise with Bayside Council to consider opportunities for nearby permanent commuter parking to offset those unavailable as a result of the changes to the Kingsgrove Avenue commuter car park e.g. Bexley North Library and Slade Road car parks</li> <li>(i) liaise with Bayside Council to consider potential opportunities for bike parking facilities and/or kiss-and-ride bay(s) where provision of these facilities is feasible</li> <li>(j) routes to be used by heavy construction-related vehicles to minimise impacts on sensitive land uses and businesses</li> <li>(k) details for relocating kiss-and-ride, taxi ranks and rail replacement bus stops if required, including appropriate signage to direct patrons, in consultation with the relevant bus operator. Particular provisions should also be considered for the accessibility impaired</li> <li>(l) measures to manage traffic flows around the area affected by the Project, including as required regulatory and direction signposting, line marking and variable message signs and all other traffic control devices necessary for the implementation of the TMP.</li> </ul>
	<p>The Proponent shall consult with the relevant roads authority during preparation of the TMP, as required. The performance of all Project traffic arrangements must be monitored during construction.</p>
37	<p><b>Road condition reports</b></p> <p>Prior to construction commencement, or as agreed with the ADEM, the Proponent shall prepare road condition surveys and reports on the condition of roads and footpaths affected by construction. Any damage resulting from the construction of the Project, aside from that resulting from normal wear and tear, shall be repaired at the Proponent's expense.</p>
38	<p><b>Road safety audit</b></p> <p>A Road Safety Audit would be undertaken as part of the detailed design process. The Road Safety Audit would include specific assessment of items including:</p> <ul style="list-style-type: none"> <li>(a) sight distances associated with the revised car parking arrangement along Kingsgrove Avenue</li> </ul> <p>The Road Safety Audit is to be submitted to and accepted by TfNSW.</p>

<b>Heritage Management</b>	
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| <b>39</b> | <b>Indigenous and non-Indigenous heritage</b><br>If previously unidentified Indigenous or non-Indigenous heritage/archaeological items are uncovered during construction works, the procedures contained in the TfNSW <i>Unexpected Heritage Finds Guideline</i> (3TP-SD-115) shall be followed and all works in the vicinity of the find shall cease. The TfNSW Environment and Planning Manager shall be immediately notified to co-ordinate a response, which may include seeking appropriate advice from a suitably qualified and experienced heritage advisor (in consultation with the Heritage Division, OEH where appropriate). Works in the vicinity of the find shall not re-commence until clearance has been received from TfNSW and/or the heritage advisor. |
| <b>40</b> | <b>Heritage Advisor</b><br>A suitably qualified and experienced Heritage Conservation Architect shall be engaged to the satisfaction of the ADEIA. The Heritage Conservation Architect will provide ongoing heritage, design and conservation advice throughout detailed design and any subsequent relevant design modifications.<br>The Heritage Advisor is required to provide specialist advice throughout the detailed design phase to ensure that the final design adheres to the recommendations of the heritage assessments provided in the EIA. The Heritage Advisor must provide evidence as to their involvement in the design process at completion of AFC design, noting how the heritage requirements listed above have been addressed in the final design.  |
| <b>41</b> | <b>Vibration Impacts to Heritage Listed Structure at the Station</b><br>To effectively mitigate potential impacts of vibration on the heritage structures within the station, activities that cause vibration are to be managed in accordance with German Standard DIN 4150 – Part 3 (DIN 1999) heritage specifications. Real time vibration monitoring is to be conducted at commencement of relevant works to confirm compliance with the German Standard DIN 4150. If vibration levels approach the determined trigger level, then the construction activity shall cease and the heritage structure is to be assessed and alternative construction methodologies developed, where practicable, before construction.  |
| <b>42</b> | <b>Archival recording</b><br>Archival recording of the station as a whole, is to be undertaken prior to the commencement of construction in accordance with the NSW Heritage Division guidelines <i>Photographic recording of heritage items using film or digital capture</i> (NSW Heritage Office, 2006) and <i>How to prepare archival records</i> (NSW Heritage Office, 1998). Copies are to be provided to Bayside Council and Sydney Trains Heritage Team for future reference. In particular, the station building and platforms, are to be comprehensively included as part of the archival recording.  |

END OF CONDITIONS